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STATUTORY INSTRUMENTS

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**1991 No. 1615**

**HIGHWAYS, ENGLAND AND WALES**

**The County Council of Somerset (Bridgwater  
Bypass) (North) (River Parrett Bridge)  
Scheme 1988 Confirmation Instrument 1991**

*Made - - - - 19th April 1991*

The Secretary of State for Transport makes this Instrument in exercise of powers conferred by section 106(3) of the Highways Act 1980(1), and now vested in him(2), and of all other enabling powers:

1. The County Council of Somerset (Bridgwater Bypass) (North) (River Parrett Bridge) Scheme 1988 is hereby confirmed without modifications.
2. The said Scheme as confirmed “the Scheme” is set out in the Schedule hereto. Copies of the Scheme with its accompanying plans are deposited at the offices of the Department of Transport, and at the offices of the Somerset County Council at County Hall, Taunton.
3. The Scheme will become operative on the date on which notice that it has been confirmed is first published in accordance with paragraph 1 of Schedule 2 to the Highways Act 1980.
4. This Instrument may be cited as the County Council of Somerset (Bridgwater Bypass) (North) (River Parrett Bridge) Scheme 1988 Confirmation Instrument 1991.

Signed by authority of the Secretary of State for Transport

19th April 1991

*Elizabeth Hopkins*  
Regional Director South West Region,  
Department of Transport

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(1) 1980 c. 66.  
(2) S.I.1981/238.

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## THE SCHEDULE

### THE COUNTY COUNCIL OF SOMERSET (BRIDGWATER BYPASS) (NORTH) (RIVER PARRETT BRIDGE) SCHEME 1988

The Somerset County Council (hereinafter referred to as “the Council” makes this Scheme in exercise of their powers under Section 106(3) of the Highways Act 1980 and of all other enabling powers:–

1. The Council are authorised to construct over the navigable waters of the River Parrett the bridge specified in Schedule 1 to the Scheme as part of the highway which they are proposing to construct from a point 30 metres east of the Bridgwater–Cannington County Road (A39) (Main Road) in a generally easterly direction to its junction with Bristol Road (A38) 945 metres south of the roundabout at Dunball at Bridgwater in the District of Sedgemoor in the County of Somerset, and the provisions contained in Schedule 2 to the Scheme shall apply for the purpose of regulating its operation.

2. This Scheme may be cited as the County Council of Somerset (Bridgwater Bypass) (North) (River Parrett Bridge) Scheme 1988.

## SCHEDULE 1

### SPECIFICATION OF THE RIVER PARRETT BRIDGE

#### Plans

1. Location Plan.
2. Site Plan
3. Elevation from the South.

The proposed bridge will be a five span structure, comprising three fixed approach spans to the east, an opening span comprising an overhead counter weight bascule located over the deep water channel adjacent to the west flood bank, and a further fixed span to the west. The fixed approach spans will be each approximately 25 metres and the bascule span will provide a clear navigation width of 9 metres.

The overall bridge length will be approximately 130 metres. The bridge will carry a single carriageway of 7.300 metres with a 2.000 metre footway on each side.

The overall width of the superstructure will be generally 12.300 metres and the soffit level will be 8.500 metres A.O.D.

The approach spans will be of composite construction, comprising steel plate girders acting compositely with a reinforced concrete deck. The three approach spans on the east side will be continuous over the intermediate piers, while the west span will be simply supported.

The bascule span will be pivoted on the main pier sited on the west side of the navigation channel and will comprise a trough stiffened steel orthotropic deck, surfaced with bauxite chippings in epoxy resin. The counterweight frame and towers will be of stiffened steel box construction.

The lifting span will be raised and lowered by hydraulic cylinders acting on a steel torsion box at the rear of the lifting leaf. The cylinders will be driven by hydraulic pumps and motors housed in a separate machinery room sited behind the west flood bank.

Each abutment will comprise a reinforced concrete bank seat supported on piled foundations. Each intermediate pier will comprise a reinforced concrete crosshead and reinforced concrete columns

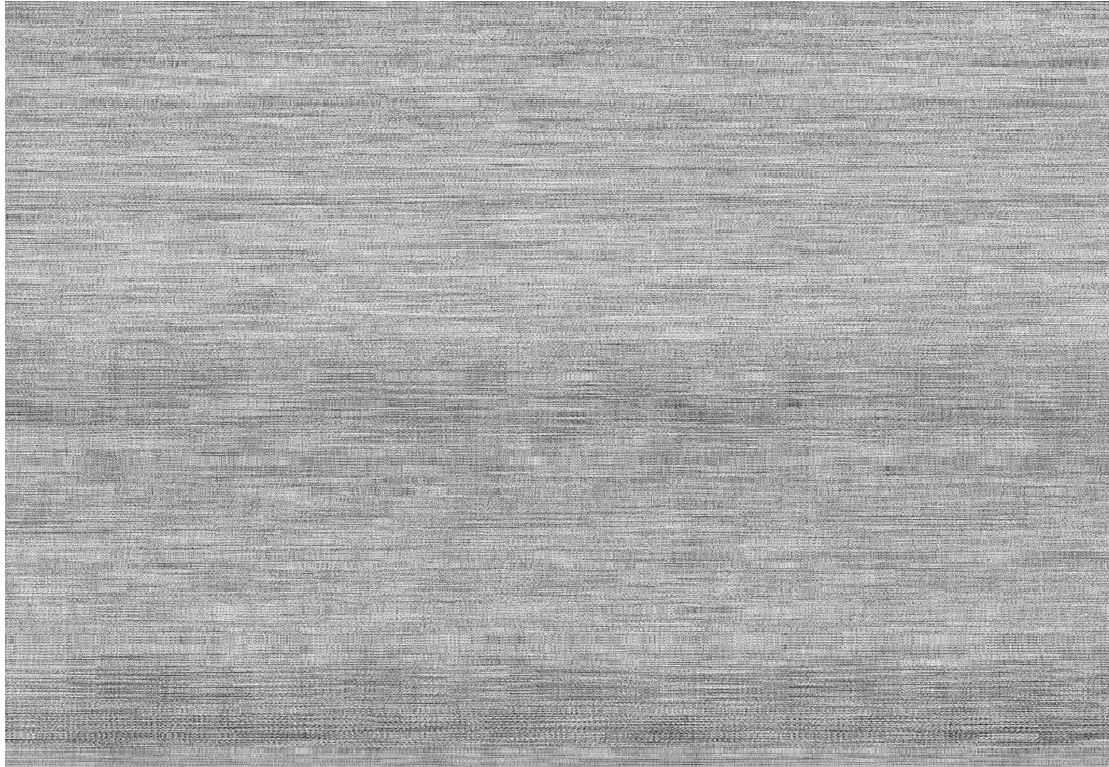
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supported on a piled base slab. The main and nose piers will be reinforced concrete structures supported on groups of piles.

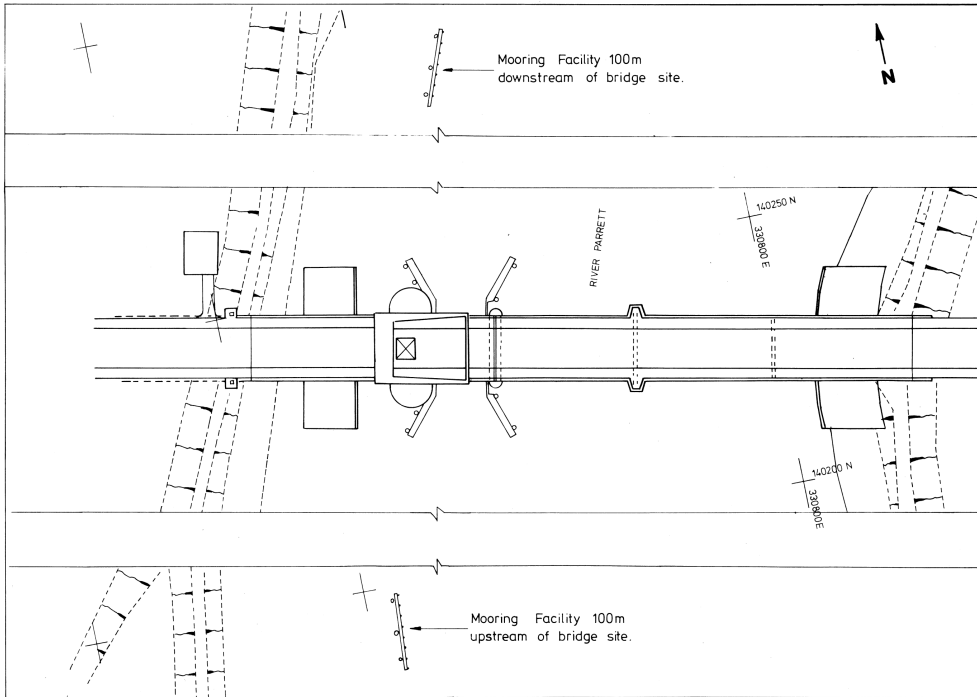
The main pier will be a two level structure, with the hydraulic cylinders being housed below road level.

Fendering will be provided to the main and nose piers, and mooring facilities will be provided approximately 100 metres upstream and downstream of the bridge.

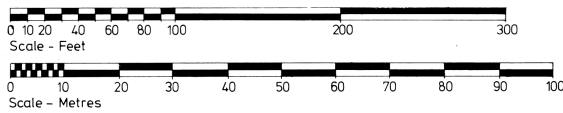
Concrete mattress protection will be provided to the flood banks in the vicinity of the bridge.



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SITE PLAN



Location:- In the Parishes of Bridgwater Without and Chilton Trinity in the District of Sedgemoor. The bridge commences on the east bank of the River Parrett [O.S. co-ordinate 330705 easting, 140243 northing] approximately 260 metres west of a point on the A38 Bristol Road some 945 metres south of the centre of the roundabout at Dunball and terminates on the west bank of the River Parrett [O.S. co-ordinate 330825 easting 140220 northing] approximately 350 metres west of the same point on the A38 Bristol Road.

Headway:- Not less than 4.20m above normal navigation level which is 4.30m above Ordnance Datum Newlyn, to be maintained under the lifting span

Waterway:- Existing profile of waterway and banks to be maintained

Span :- There are 5 no spans over the river providing a minimum width of waterway for navigation of 90 metres

**SCHEDULE 1**

**RIVER PARRETT BRIDGE**

## SCHEDULE 2

### PROVISIONS FOR REGULATING THE OPERATION OF THE BRIDGE

The bridge will be open free of charge for the passage of vessels for such reasonable periods as may be required on every day between the hours of sunrise or 0600 hours (whichever is the later) and sunset or 2200 hours (whichever is the earlier) and at all other times the bridge will be closed against vessels except in case of emergency or after prior arrangement with the bridge operator.

Given under the Common Seal of the Council the twelfth day of December, One thousand nine hundred and eighty-eight.

The Common Seal of the Council was hereunto affixed in the presence of:–

L.S.

Senior Assistant Solicitor