
STATUTORY INSTRUMENTS

1986 No. 1856

HIGHWAYS, ENGLAND AND WALES

The Highways (Road Humps) Regulations 1986

<i>Made - - - -</i>	<i>23rd October 1986</i>
<i>Laid before Parliament</i>	<i>10th November 1986</i>
<i>Coming into Operation</i>	<i>1st December 1986</i>

The Secretary of State for Transport as respects England and the Secretary of State for Wales as respects Wales, in exercise of the powers conferred by sections 90C(1) and 90D(1) and (2) of the Highways Act 1980 (a), and all other enabling powers, and after consultation with representative organisations in accordance with sections 90C(6) and 90D(3) of that Act, hereby make the following Regulations:—

Citation and commencement

1. These Regulations may be cited as the Highways (Road Humps) Regulations 1986 and shall come into operation on 1st December 1986.

Interpretation

2.—(1) In these Regulations—

“the Act” means the Highways Act 1980;

“hours of darkness” means the time between half-an-hour after sunset and half-an-hour before sunrise;

“principal road” means a highway which is a road for the time being classified as a principal road by virtue of section 12 of the Act (whether as falling within subsection (1), or as being so classified under subsection (3), of that section);

“road” has the same meaning as in section 196(1) of the Road Traffic Act 1972 (b);

“traffic sign” has the same meaning as in section 64 of the Road Traffic Regulation Act 1984 (c);

“the Traffic Signs Regulations” means the Traffic Signs Regulations and General Directions 1981 (d)

(2) For the purpose of these Regulations road humps in a highway shall be deemed to form part of a series where they are two or more in number and spaced so that not less than 20 metres nor more than 150 metres lies between one hump and the one next to it.

(a) 1980 c.66; sections 62 and 329 were amended, and sections 90A to 90F were inserted, by section 32 of, and Part I of Schedule 10 to, the Transport Act 1981 (c.56).

(b) 1972 c.20.

(c) 1984 c.27.

(d) S.I. 1981/859; the relevant amending instruments are S.I. 1982/1879 and 1880, 1983/1086 and 1088, 1984/966 and 1986/1859.

(3) For the purposes of these Regulations measurements to or from any point to or from a road hump shall be taken to or from that edge of the hump, as shown in the Schedule, which is nearest to the point and for the purposes of regulations 4 and 6 and of this paragraph:—

- (a) measurements to or from a horizontal bend shall be taken to or from that part of the bend which is nearest to the hump;
- (b) measurements to or from a road junction shall be taken to or from the point nearest the hump on an imaginary line drawn across the mouth of the junction; and
- (c) a horizontal bend begins where a vehicle would start the change of direction specified in regulation 4(2)(b)(i) and ends where a vehicle would finish that change of direction.

(4) A reference in these Regulations to a numbered regulation is a reference to the regulation bearing that number in these Regulations and a reference to the Schedule is a reference to the Schedule to these Regulations.

Consultation about road hump proposals

3. Where the Secretary of State or a local highway authority proposes to construct a road hump, he or they shall, as well as consulting the chief officer of police as required by section 90C(1) of the Act, also consult:—

- (a) where the proposal is by a local highway authority other than a London Borough Council or a Metropolitan District Council the district council in whose district the highway is situated;
- (b) in all cases, one or more organisations representing persons who use the highway to which the proposal relates, or representing persons who are otherwise likely to be affected by the road hump, unless it appears to the Secretary of State or the local highway authority that there are no such organisations.

Highways and circumstances in which road humps may be constructed

4.—(1) A road hump shall not be constructed in—

- (a) any highway which has more than one carriageway, or
- (b) any principal road, trunk road or special road whether or not it has more than one carriageway.

(2) A road hump shall only be constructed in a highway where:—

- (a) to afford illumination throughout the hours of darkness there is a system of street lighting furnished by at least three lamps lit by electricity and placed not more than 38 metres apart or external lighting specifically provided for the hump or a street lamp within 5 metres of the hump; and
- (b) on each road affording access for vehicular traffic to the road hump or, in the case of a series of road humps, the first in the series to be met by such traffic, there is a feature of one or more of the following descriptions—
 - (i) save as provided in regulation 5(1) and (2) a horizontal bend or a junction between one road and another at which vehicular traffic would change its direction by not less than 70 degrees within an inner kerb radius of not more than 25 metres,
 - (ii) a traffic sign facing away from the road hump conveying to vehicular traffic the prohibition specified in diagram 616, 617 or 619 in Part II of Schedule 1 to the Traffic Signs Regulations,

(iii) the end of the carriageway at the closed end of a cul-de-sac,
(iv) a traffic sign conveying to vehicular traffic the prohibition specified in diagram 1002.1 in Schedule 2 to those Regulations
any such feature is in the case of (i), (ii) or (iii) within 40 metres and in the case of (iv) within 60 metres of the hump.

Relaxations to regulation 4

5.—(1) Notwithstanding the provisions of regulation 4 (2) (b) (i), where in a length of highway there are:—

- (a) two side road junctions not more than 25 metres apart, or
- (b) three side road junctions none of which is more than 25 metres from the next one

a road hump may be constructed beyond each end of that length of highway if each hump is constructed within 15 metres from the side road junction nearest to it.

(2) Notwithstanding the provisions of regulation 4(2)(b) (i) (ii) and (iii), where in a length of highway starting with a feature of a kind specified in regulation 4(2)(b) (i), (ii) or (iii):—

- (a) there is a side road junction situated not more than 25 metres from that feature, or
- (b) there are two side road junctions not more than 25 metres apart and one or both of which is not more than 25 metres from that feature

a road hump may be constructed beyond the end of that length of highway furthest from that feature if the hump is constructed within 15 metres from the junction or from the junction furthest from the feature (as the case may be).

(3) For the purposes of this regulation a side road junction includes a cross-road junction.

Nature, dimensions, location and spacing of road humps

6.—(1) A road hump constructed or maintained in a highway shall be so constructed and maintained that—

- (a) save as provided in paragraph (3) below, it extends across the whole width of the carriageway on which it is placed and its cross-section conforms to the pattern and measurements shown in the Schedule, a measurement being deemed to conform if it is, in the case of a vertical measurement, no more than 15 per cent or 12 millimetres greater or no more than 20 per cent or 12 millimetres less, and, in the case of a horizontal measurement, no more than 5 per cent, greater or less, than the corresponding measurement so shown;
- (b) no part of it is within 8 metres of a road junction giving access to vehicular traffic to that highway or a horizontal bend with an inner kerb radius of less than 50 metres at which a vehicle would change its direction by more than 45 degrees;
- (c) it is at right angles to an imaginary line along the centre of the carriageway of the highway in which it is constructed;
- (d) it is not within 500 metres of another road hump in that highway unless it forms part of a series with that other road hump; and

- (e) where it is constructed or maintained in a carriageway with a gradient of more than 10 per cent, it is not within 20 metres of the top of that part of the carriageway which has that gradient and the distance between road humps on the gradient is not greater than 70 metres.
- (2) A road hump may be constructed and maintained so that its centre line, at right angles to the edge of the road, is in the same position as the centre line of the pattern of black and white stripes specified in paragraph 4(1) of the "Zebra" Pedestrian Crossings Regulations 1971(a).
- (3) Save as mentioned in paragraph (2) above, a road hump shall not be constructed or maintained in a highway within the limits of a zebra controlled area as defined in regulation 3(1) of the said Regulations of 1971 or within 30 metres from the centre line, at right angles to the edge of the road, of the said pattern of stripes.
- (4) A road hump may not be constructed or maintained within the limit of a "pelican" crossing as defined in regulation 3(1) of the "Pelican" Pedestrian Crossings Regulations 1969(b) or within 30 metres of any part of any such crossing.
- (5) A road hump shall not be constructed or maintained in a highway:—
 - (a) on a railway level crossing or within 20 metres of the nearest rail forming part of the railway track at any such crossing;
 - (b) within 20 metres of any part of any traffic sign of a kind shown in diagram 845, 846, 847, 848, 849, 850, 851 or 852 in Part IV of Schedule 1 to the Traffic Signs Regulations or within 10 metres of a traffic sign of a kind shown in diagram 1025, 1025.1, 1025.2 or 1025.3 in Schedule 2 to those Regulations.
 - (c) under or within 25 metres of a structure over a carriageway of the highway any part of which is 6.5 metres or less above the surface of the carriageway;
 - (d) where it would form part of a series of more than 20 humps; or
 - (e) above or within 25 metres of any part of a bridge, tunnel, culvert or other similar structure over which that highway passes.

Placing of traffic signs

7. Where a road hump or series of road humps is constructed in a highway the highway authority for that highway shall forthwith cause to be placed and thereafter cause to be maintained:—

- (a) traffic signs of a kind shown in diagram 557.1, in combination with traffic signs of a kind shown in diagram 557.2, 557.3 or 557.4 in Schedule 1 to the Traffic Signs Regulations in such positions as the authority may consider requisite for the purpose of securing that adequate warning of the presence of a road hump or a series of road humps is given to persons using the highway; and
- (b) except in a case specified in sub-paragraph (c) below, on the road hump or, in the case of a series of road humps, on each hump in the series, the traffic sign shown in diagram 1060 in Schedule 2 to those regulations; and
- (c) in a case where a road hump is placed within the area of a pedestrian crossing which complies with the requirements prescribed in the

(a) S.I. 1971/1524.

(b) S.I. 1969/888.

“Zebra” Pedestrian Crossings Regulations 1971, a traffic sign of the kind shown in diagram 544 in Part I of Schedule 1 to the Traffic Signs Regulations to face traffic approaching that crossing together with a traffic sign of the kind shown in diagram 547.5 in that Part.

8. The Highways (Road Humps) Regulations 1983(a) are hereby revoked.

John Moore,
Secretary of State for Transport.

23rd October 1986.

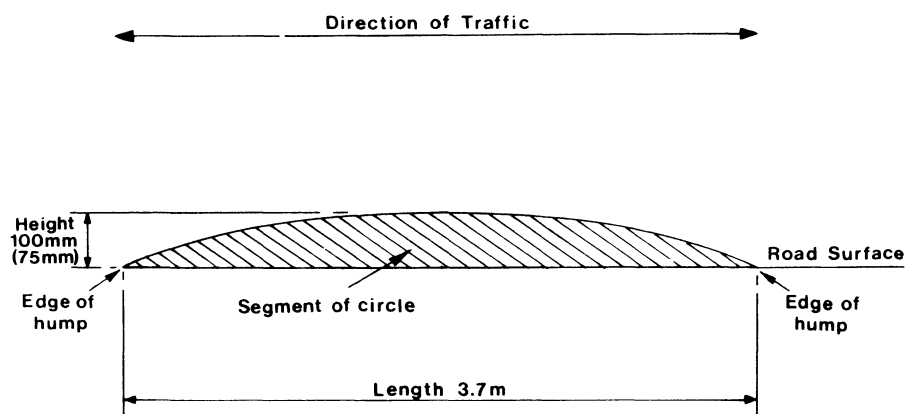
Nicholas Edwards,
Secretary of State for Wales.

23rd October 1986.

SCHEDULE

(See Regulation 6)

CROSS SECTION AND HUMP DIMENSIONS



(a) S.I. 1983/1087.

EXPLANATORY NOTE

(This Note is not part of the Regulations.)

1. These regulations revoke the Highways (Road Humps) Regulations 1983 (regulation 8) and replace the provisions of those regulations.

2. Regulation 2 deals with interpretation. Regulation 3 contains provisions for consultation about road hump proposals. Regulation 4 provides, in paragraph (1), that a road hump shall not be constructed in a principal road, a special road, a trunk road or dual carriageway road and, in paragraph (2), that a road hump shall only be constructed (save as provided in regulation 5) in a highway which is lit as there specified and where one or more of the features there specified exists. Regulation 5 contains exemptions from the provisions of regulation 4. Regulation 6 and the Schedule prescribe the nature, dimensions, location and spacing of road humps. Regulation 7 prescribes traffic signs to be placed and maintained in connection with road humps.

3. The main differences between these regulations and the revoked regulations are that provisions in relation to bus routes are deleted, provision is made for humps to be placed in the pattern of black and white stripes specified in paragraph 4(1) of the "Zebra" Pedestrian Crossings Regulations 1971, the requirements about the attendant traffic signs are amended and the requirements about the dimensions are amended.

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